



Lakewood Heights

Lakewood Heights : MRC-1 Ordinance



Department of Planning and Community Development

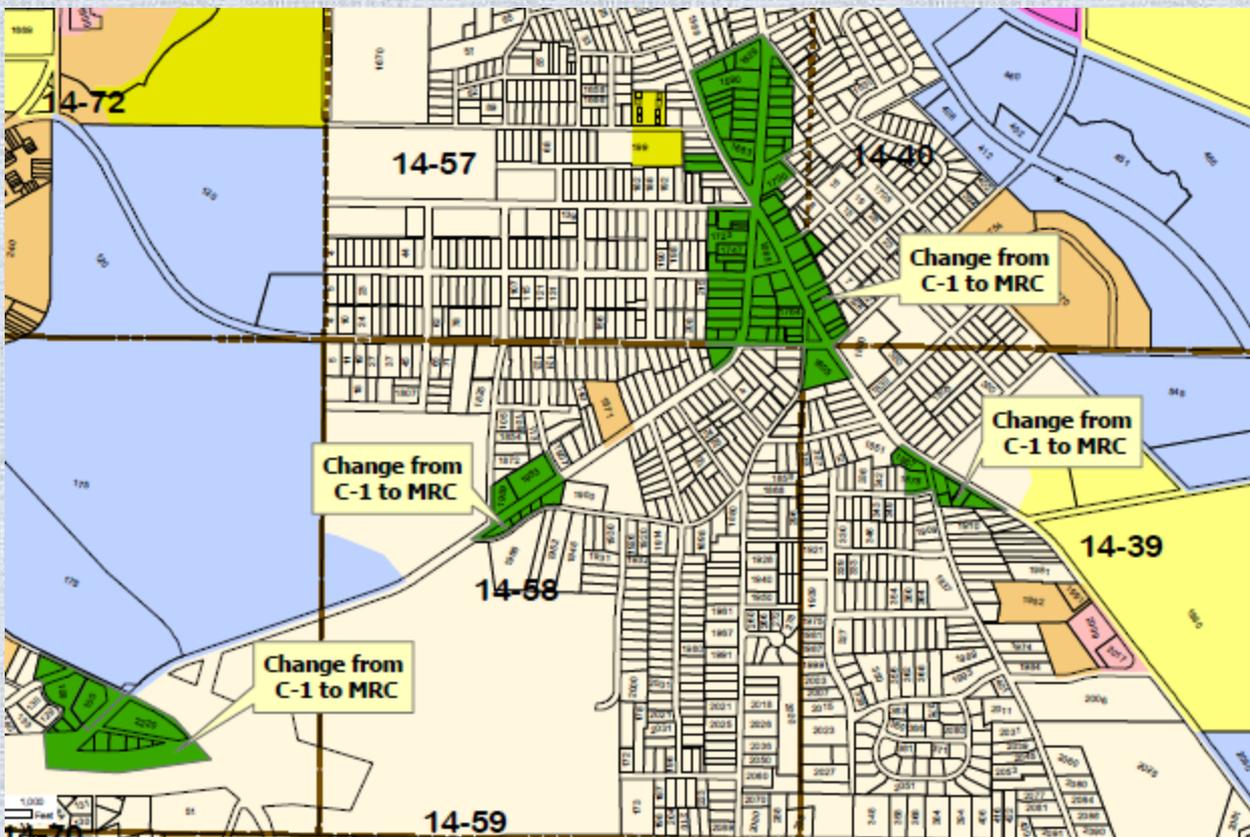
**Charletta Wilson Jacks, Director
Office of Zoning and Development Review**

8/10/2016



Lakewood Heights

Commercial Center rezoning area



Our study area lies in district 1.

From **Lethea street on the north to Macon drive on the southwest and Adelle street on the southeast.**



Existing Condition: Town Center (Intersection of Jonesboro and Lakewood)



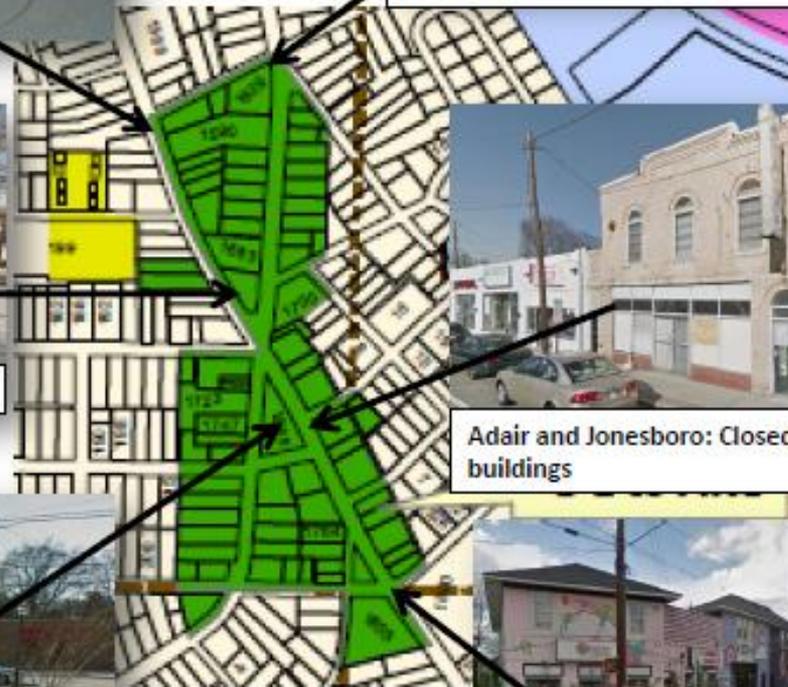
Lethea and Jonesboro: Postal Service



Lethea and Lakewood: Out migrated business – potential for MRC



Lakewood and Jonesboro (North): Gas Station



Lakewood and Jonesboro (South): Family Dollar and a restaurant



Adair and Jonesboro: Closed businesses and empty buildings



Claire and Jonesboro: Abandoned – but have potential to be iconic structure for the community

Existing Condition: Southwest and Southeast Commercial nodes



Lakewood and Macon: Gas station



Lakewood and Shadydale: Gas station, with MRC this area can be more livable



Lakewood and Macon: Land for Sale!



Jonesboro and Brownsmill: Gas station

Total number of gas stations and distance between them

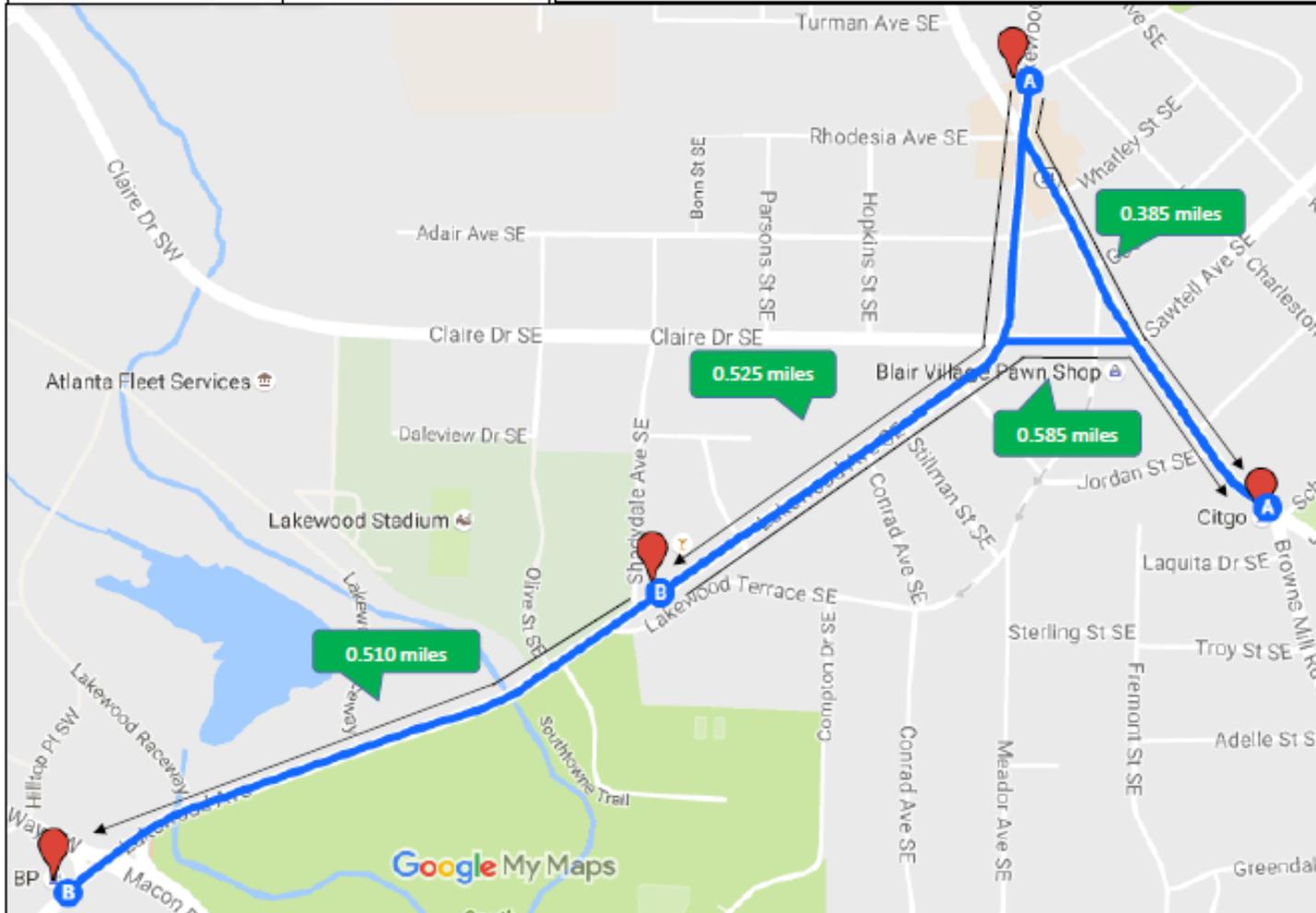
Gas Station	Distance
Chevron to Citgo	0.385 miles
Chevron to Exxon	0.525 miles
Exxon to Citgo	0.585 miles
Exxon to BP	0.510 miles

Would you like to see a limit of the # of gas stations in this area?

Would you like to see a specific distance between gas stations in the area?

Where would you like the gas stations to be located?

Do you want to prohibit more gas stations from coming into the area?



Summary of public comments from first public meeting held on July 28, 2016

List of concerns

- Vacant land
- Not livable
- Large convenient stores and gas stations as opposed to small business
- Not walkable
- Lack of interesting stores (Restaurants, corner stores, coffee shops etc.)
- Lack of barber shop
- Lack of health care building services in the community
- Presence of unwanted businesses such as drugs, prostitution etc.

Why MRC?

Proponents of smart growth argue that many of these problems can be addressed by mixing complementary land uses.

- Separated land use is defining characteristic of **sprawl that isolated residential areas from shopping, services, and employment.**
- Excessive commute times, traffic congestion, air pollution
- Inefficient energy consumption,
- Loss of open space and habitat,
- Inequitable distribution of economic resources, job/housing imbalance, and reduced sense of community.
- Variety of dwelling types appeal to different ages and incomes; improves livability and a sense of place;
- Helps reduce car dependence; and makes other forms of transportation viable.
- Increased pedestrian activity not only helps revitalize community life and enhances security but also provides foot traffic to support local businesses.
- Strong sense of place also attracts outsiders

Code of Ordinances

MRC MIXED RESIDENTIAL COMMERCIAL DISTRICT REGULATIONS

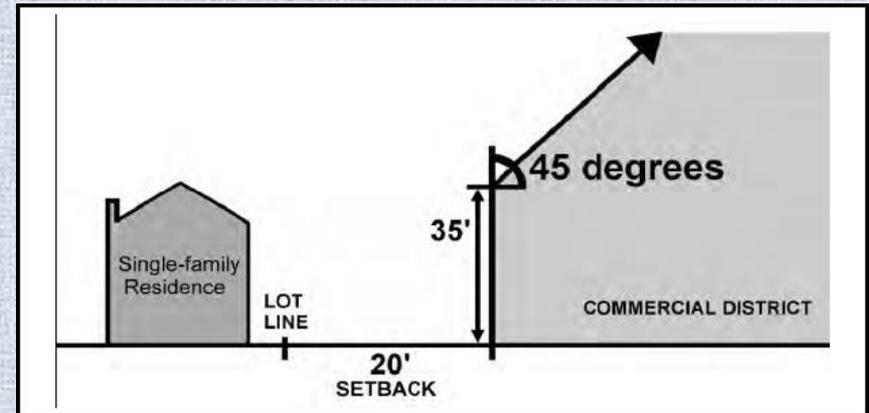
Code of Ordinances

- Sec. 16-34.005. - Permitted principal uses and structures.
- Sec. 16-34.006. - Permitted accessory uses and structures.
- Sec. 16-34.007. - Special permits.
- Sec. 16-34.009. - Transitional uses and yards.
- Sec. 16-34.010. - Development controls.
- Sec. 16-34.011. - Site limitations.
- Sec. 16-34.012. - Sidewalks.
- Sec. 16-34.013. - Supplemental zone.
- Sec. 16-34.014. - Relationship of building to street.
- Sec. 16-34.015. - Signage.
- Sec. 16-34.016. - Loading areas, loading dock entrances and building mechanical and accessory features.
- Sec. 16-34.017. - Off-street loading requirements.
- Sec. 16-34.018. - Curb cuts and parking structures.
- Sec. 16-34.019. - Lighting, security, and maintenance requirements.
- Sec. 16-34.020. - Minimum landscaping for surface parking lots, barrier requirements.
- Sec. 16-34.021. - Off-street parking requirements.
- Sec. 16-34.022. - Membership in transportation management associations and transportation management plans.
- Sec. 16-34.023. - Minimum bicycle parking requirements.
- Sec. 16-34.024. - Pedestrian bridges and tunnels.
- Sec. 16-34.025. - Zero-lot-line development.
- Sec. 16-34.026. - Specific regulations for MRC-1.

Transitional uses and yards

Transitional Height Plane

- Height plane shall begin 35 feet above the buildable area boundary and extend at an angle of 45 degrees away from a single family residence

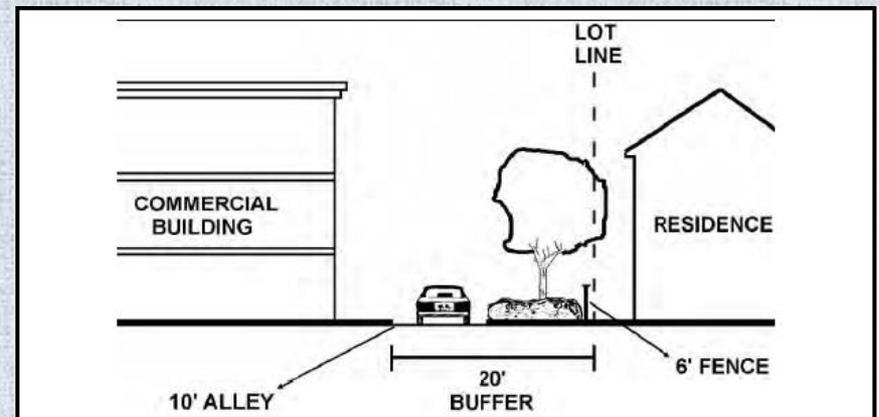


Transitional Yard Screening

- Buffers shall include permanent opaque walls 6 feet in height

Transitional Yard

- Where this district adjoins an R-1 through R-5, R-G, MR, RLC or PD-H district without an intervening street
- A 20 feet wide landscaped buffer located adjacent to residentially zoned properties
- Buffers may accommodate a 10 foot alley



Development Controls

- **Non residential: Amount of area that shall be public space**
 - If Less than or equal to one-half acre: A minimum of 10% of the net lot
 - If more than half acre: 20% of the net lot
- **In Mixed Use storage** : At least 10% should be non-storage use(s)
- **Maximum building coverage**: 85 % of the net lot area
- **Bulk limitation**: 20 % of total floor area

- **Side or rear yard**
 - For residential uses: **a minimum 20 feet side** and rear yard setback shall be required, except that the **side yard may be reduced to zero feet** when a residential use has no residential windows adjacent to such yard.
 - For nonresidential uses: No requirement.
- **Residential balconies** may be counted towards UOSR for a max. depth of six feet.
- **Affordable requirements** shall be in place for a minimum of 20 years.

Development Controls

New Street Incentive

New streets can count towards UOSR when:

- Connects 2 public streets
- Meets sidewalk requirements
- Max. width of 36 feet and includes 2 travel lane, 2 parallel parking lanes and sidewalks with granite curbs
- No gates across street

On-street parking can count as open space when:

- Sidewalk extensions are provided at street intersections
- No on-street parking is currently provided
- The development is an entire block face or a minimum of 200 ft.

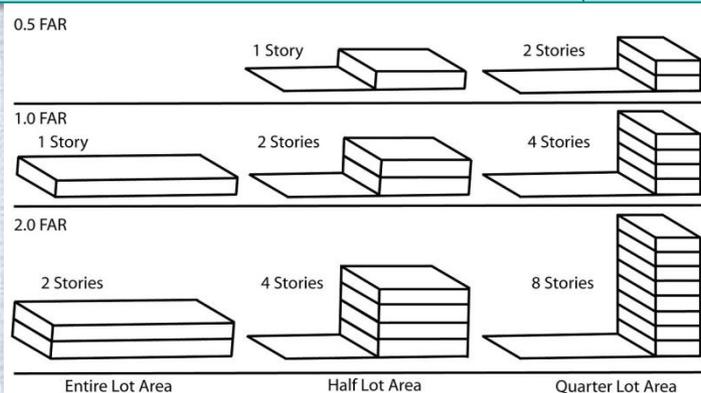


Maximum permitted floor areas with bonuses

- Maximum permitted floor areas with bonuses. Under no circumstances shall the floor area of any development with bonuses exceed an amount equal to two and six hundred ninety-six thousandths times gross lot area.
 - *Open space and streets bonus.* Developments shall be permitted a bonus equal to two additional square feet of residential floor area for each one square feet of open space provided above the minimum required herein without the bonus.
 - *Affordable housing bonus.* Developments containing residential uses shall be permitted a floor area bonus of one times net lot area, provided that a minimum of 20 percent of the total floor area developed shall be used for affordable sales housing units or affordable rental housing units.
 - *Civic bonus.* Developments which provide buildings housing recreational centers, community centers and community service facilities which are available to the general public during normal city recreational center, community center or community service hours shall be permitted a residential floor area bonus equal to the total recreational center, community center or community service facility floor area.

Summary of Density and Open Space Requirements

Without bonus Base FAR- (Floor to net lot area ratio)		
Nonresidential	Residential	Combined
1.0	0.696	1.696
Bonus FAR* (net lot area)		
Residential (Open Space & Streets bonus)	Residential(Affordable Housing [^])	Residential(Civic)
Two sq. ft. of residential floor area for every one sq. ft. of open space	1.0	One sq. ft. of residential floor area for every one sq. ft. of civic space



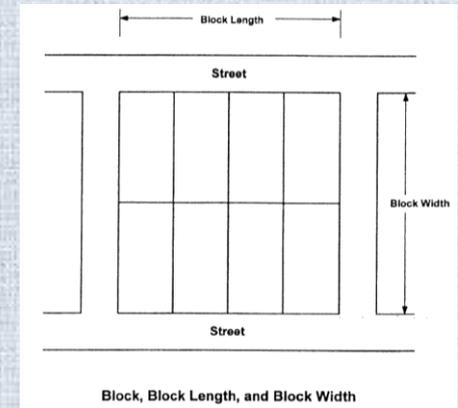
Floor area ratio (FAR) is the **ratio** of a building's total **floor area** (gross **floor area**) to the size of the piece of land upon which it is built.

$$\text{FAR} = \frac{\text{Total floor area}}{\text{net lot area}}$$

FAR illustration

Site limitations

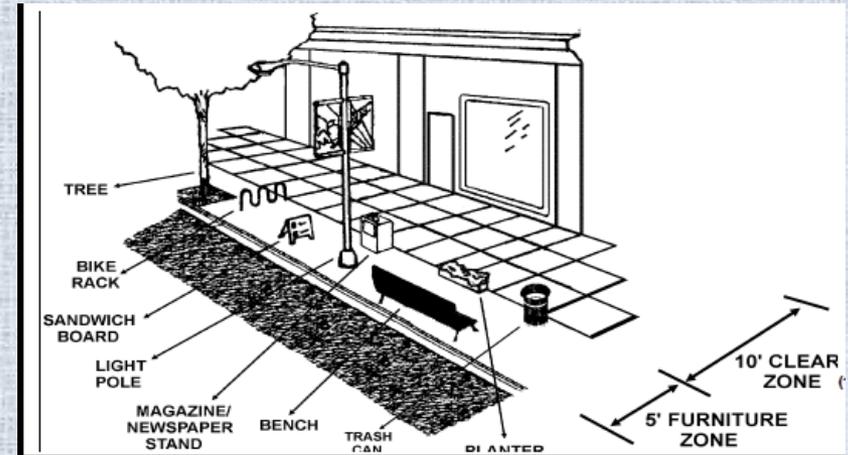
- If entire block has face greater than 600 feet in length, it shall be traversed by streets which create block faces no more than 400 feet in length
- Properties adjacent to the right-of-way of public streets which dead-end or cul-de-sac, shall provide a street connection to said right-of-way of public street
- Properties adjacent to a public space such as a park space, greenway trail, railroad right-of-way which has been abandoned or a railroad right-of-way with an existing or proposed rail-trail:
 - Minimum 20-foot wide buffer and completely landscaped
 - Provide a new public access street, pedestrian walkway, or bike and jog path between any development
 - Shall not locate off-street parking areas or loading docks between any building



Sidewalks

Sidewalk Widths

- Sidewalks shall be a minimum of 15 feet which includes a 10 feet wide clear zone
- The street furniture zone shall be adjacent to the curb



Street Tree Placement

- Street trees planted a distance of 25 - 40 feet apart
- Planting area per tree a minimum of 25 square feet



Supplemental Zone

the area between any building, parking garage, or parking lot and the required sidewalk, when no intervening building exists

Mixed-Use Supplemental Zone

- Located between the sidewalk and the building facade
- Hardscaped and used for public plazas, terraces or accessory outdoor dining
- Accessory outdoor dining may be separated from the sidewalk only with movable planters or fencing with a maximum height of 36 inches

Residential Supplemental Zone

- Must be landscaped or provide a porch/terrace
- Walls maximum height of 24 inches, except retaining walls, which shall not exceed a maximum height of 36 inches

Fencing permitted only when:

- The supplemental zone is located between sidewalk level residential units and the adjacent street; or
- fencing is used to separate authorized outdoor dining from the required sidewalk.



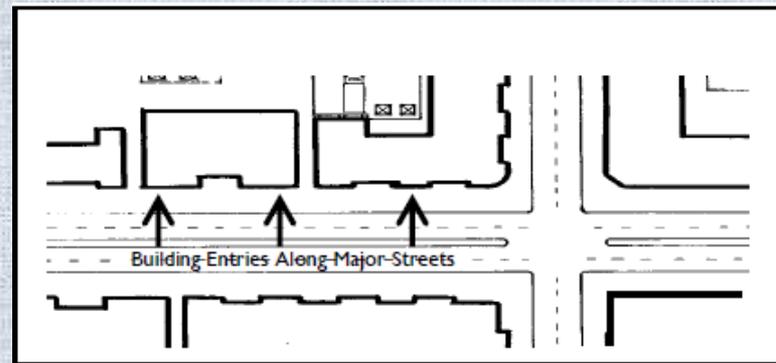
Relationship of building to street

Floor Delineation

- Belt courses, cornice lines, or similar architectural detailing shall delineate the first three building floors

Building Entrances

- Face and be visible from the street
- Directly accessible from the public sidewalk
- Open directly onto the adjacent public sidewalk, supplemental zone or an outdoor dining area or plaza
- Provide individual entrances when there are more than 4 ground floor residential units



Relationship of building to street

- **Sidewalk arcades. Buildings with nonresidential uses**

- Shall provide an at-grade sidewalk surface.
- Arcade supports shall be a maximum width of five feet.
- Shall provide a minimum of 25 feet of clear unobstructed space between arcade supports.



- **Along arterial streets and collector streets,**

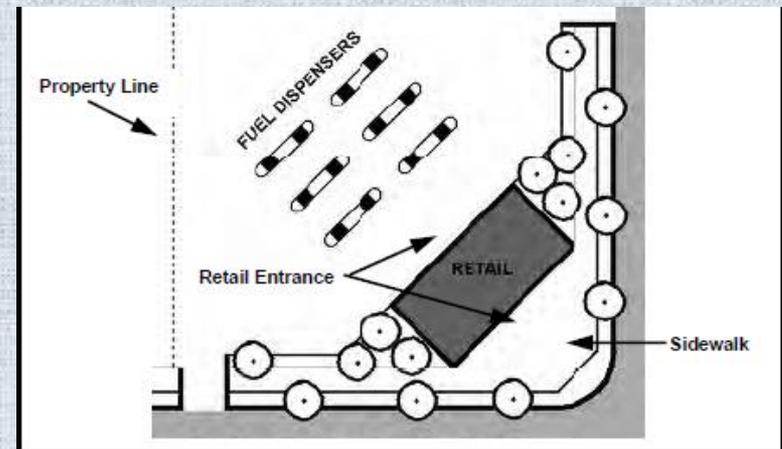
- sidewalk level uses with street frontage shall only be retail, office, institutional, or residential.
- Fenestration- Non residential:
 - for a minimum of 75 percent of the length of all street frontages

- **For streets other than arterial streets and collector streets**

- Fenestration shall be provided for a minimum of 50 percent of the length of the street frontage

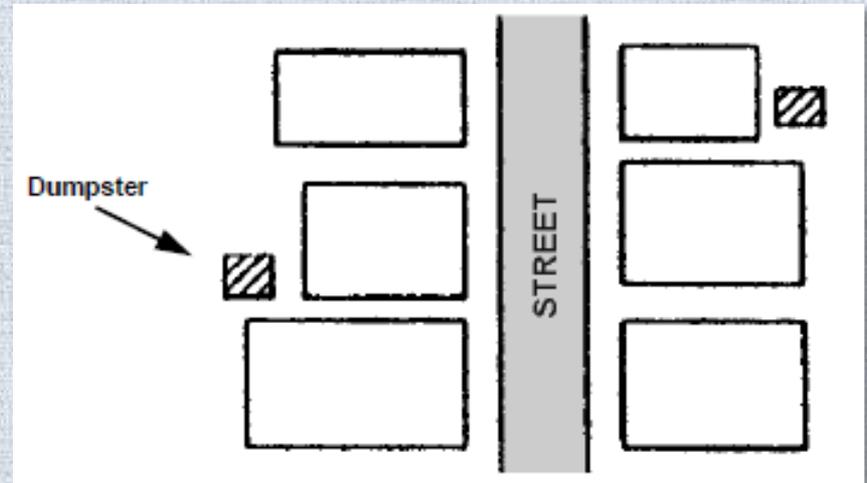
Relationship of building to street

- **Buildings with residential uses at the sidewalk level**
 - All primary pedestrian entrances not adjacent to a public sidewalk shall be linked to the public sidewalk with a pedestrian walkway a minimum of five-foot wide.
 - Such buildings shall have windows on each street frontage façade
- **Fences and walls**
 - fences shall not exceed 42 inches in height (Residential)
 - Prohibited (Non residential)
 - No barbed wire, razor wire, chain link fence or similar elements shall be visible from any public plaza, ground level or sidewalk level outdoor dining area or public right-of-way.
- Gasoline fuel dispenser structures and associated vehicular services such as air pumps and car washes shall not be located between a building and the street.



Loading areas, loading dock entrances and building mechanical and accessory features

- Shall be paved with impervious materials and shall be screened so as not to be visible from any public plaza
- Building mechanical and accessory features.
 - Shall be located to the side or rear of the principal use
 - least visibility from the public right-of-way.
 - Screening with plant or fence materials
 - Shall not be permitted between the building and any public street.

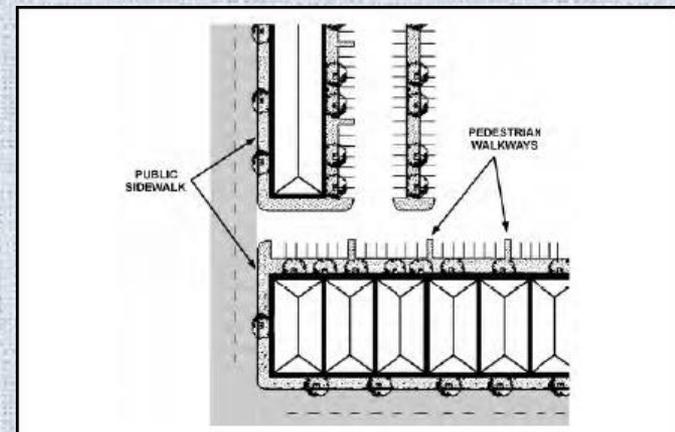
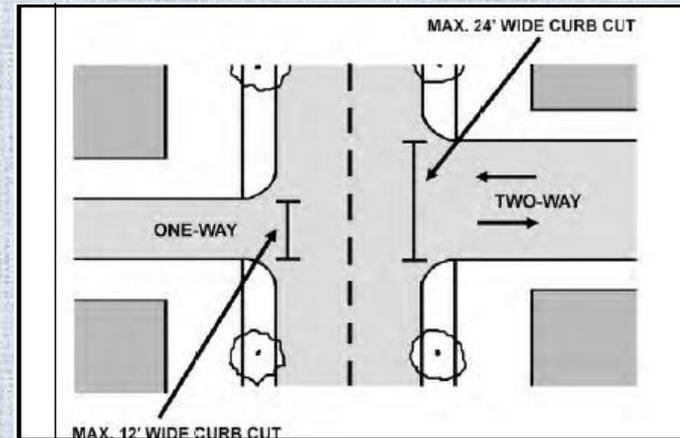


Curb cuts and parking structures.

- Driveway and curb cut widths : 24 feet for two-way entrances and 12 feet for one-way
- No circular drives shall be located except hotels
- driveways, except for a driveway to reach the an on-site parking facility,
 - are not permitted between the sidewalk and a building,
- no more than one curb cut shall be permitted for each development
- All contiguous ground-floor residential units shall share one common drive

Parking Connections

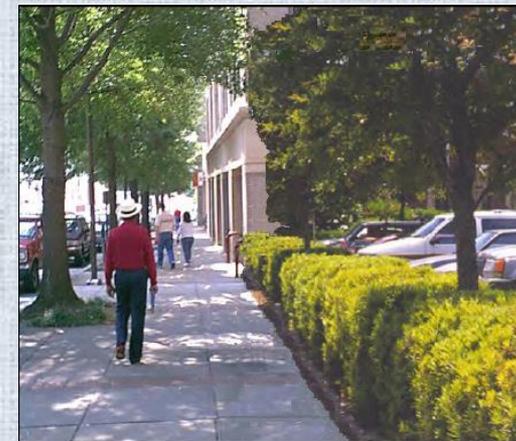
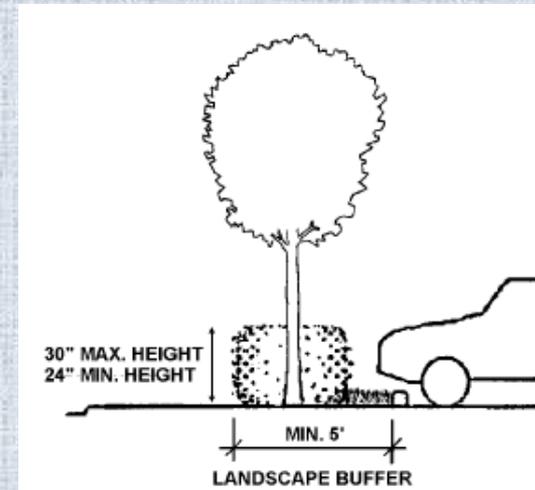
- Sidewalk: a minimum width of four feet connecting ground level parking to the public sidewalks and to all building entrances.



Minimum landscaping for surface parking lots, barrier requirements

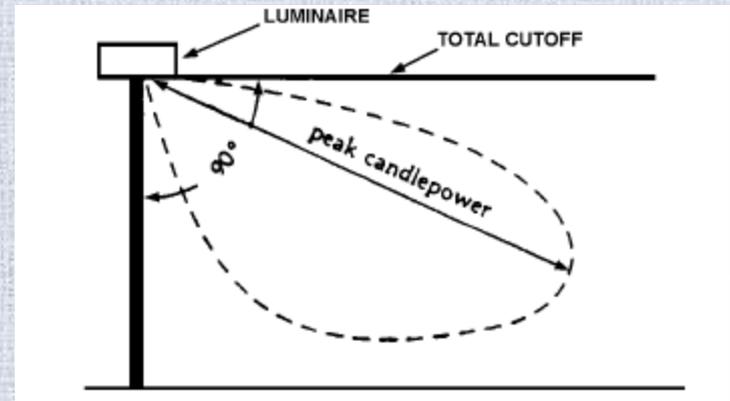
Landscaped Buffers

- Surface parking shall have a continuous landscape buffer
- At least 1 tree planted in the landscaped buffer
- Trees shall be planted a minimum of 34 feet apart
- Shrubs shall have a minimum mature height of 24 inches and a maximum height of 30 inches



Lighting, security, and maintenance requirements

- All lighting including all parking decks and lots and lit canopies shall reduce light spillage onto residentially used properties by providing cutoff luminaires which have a maximum 90 degree illumination
- All lighting that up-lights trees, buildings or other elements, shall be located a minimum height of eight feet above the sidewalk, driveway or pedestrian area when not located within completely landscaped areas.
- All surface parking lots and structures
 - Lighting shall be provided throughout all parking facilities
- Parking facilities shall be maintained in a clean, safe and sanitary condition.

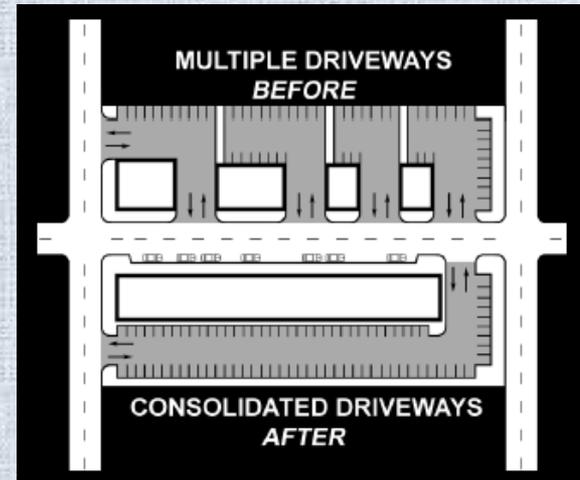


Off-street parking requirements

- Off-street surface parking shall not be located between a building and the street without an intervening building
- parking spaces serving two principal permitted use may use such facility for shared parking during non-normal business hours
- For offices:
 - at least five percent of the employee parking spaces "Carpool Only."
 - All new parking structures shall be built to accommodate vanpool access.

Electric Vehicle Charging

- All parking facilities shall include one electric charging station for each 100 automobile parking spaces.
- No development is required to exceed 5 charging stations



Off-street parking for nonresidential

Banks, savings and loan institutions, and the like	One space per 200 sq. ft.
Business colleges, trade schools, conservatories, dancing schools, and the like	One space per 200 sq. ft.
Day care centers for young children and like	One space per 600 sq. ft.
Clubs, lodges, and commercial recreational centers	One space for each 200 sq. ft.
Dormitories:	No parking requirements
Eating and drinking establishments, including accessory outdoor dining covered with a permanent structure *	One parking space per 100 sq. ft.
retail and eating and drinking establishments within 500 feet of a MARTA rail station entrance:	floor area of 500 square feet or less shall have no parking requirements.
Office **	No min. Max: 2 and ½ spaces per 1,000 sq. ft.
Fraternities, sororities:	One space for two beds plus one space for each 200 square feet for other than sleeping area
Hotels and motels***	One space per rental unit + one-half space per employee;
Nursing homes, convalescent homes, and similar care facilities:	One space for four beds.
Photocopying shops:	One space for each 200 square feet of floor area.
For all other nonresidential uses: One parking space for each 600 square feet of floor area shall be provided on the site	

Membership in transportation management associations

Office component **greater than 100,000 square feet of total gross leasable floor area of space shall become a member** with the criteria listed below.

Commute alternatives.

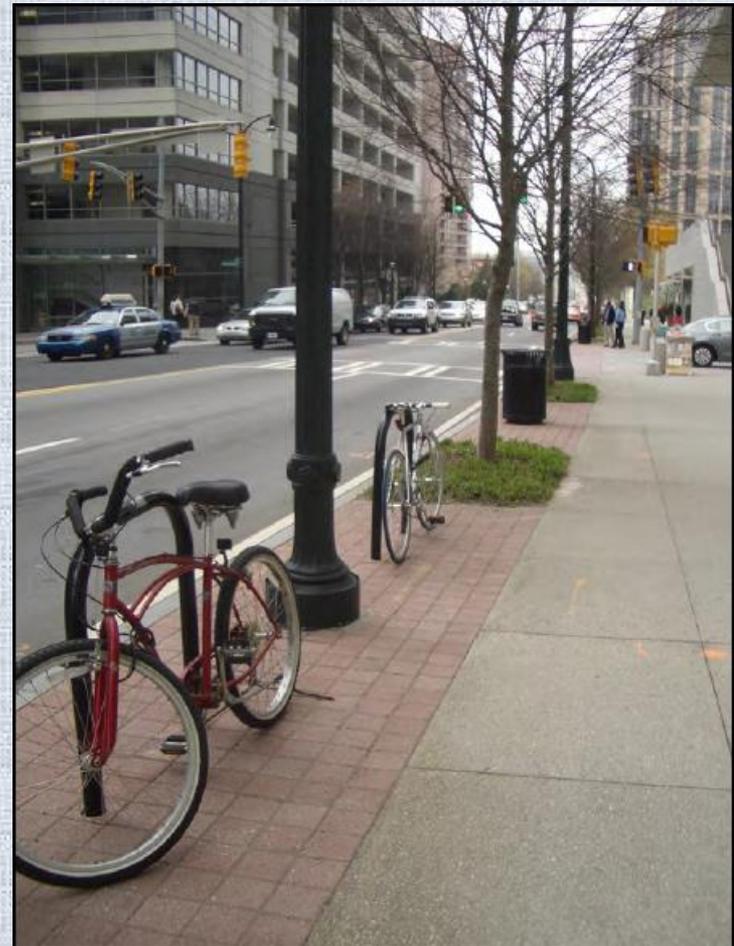
- Incentives for public transit ridership such as transit cards;
- Carpooling and vanpooling;
- Commuter bicycling and walking programs;
- Alternative work hours:
 - Staggered work hours;
 - Compressed work weeks;
 - Flexible work hours (flextime)
 - Telecommuting

Transportation demand strategies.

- Improvements to alternative modes such as vanpooling;
 - Financial incentives given to employees who use commute alternatives;
 - Parking management programs;
 - Commute alternatives information and marketing
 - Shared parking arrangements;
 - Provision for a mixture of uses on-site;
 - Pedestrian links to adjacent uses.
- A program to promote and maintain employee participation in carpooling, vanpooling, and use of mass transit, including a method of monitoring the number of ride sharers and their travel patterns

Minimum bicycle parking requirements

- All nonresidential developments which provide automobile parking facilities:
 - a ratio of at least one space for every 20 automobile parking spaces.
- Multi-family developments:
 - a ratio of at least one bicycle/moped parking space for every five multi-family units
- No development should be required to exceed a maximum of 50 spaces



Signage

- On institutional property, one sign not exceeding 35 square feet in sign area is permitted
- Signs Not to Constitute Traffic Hazard
- Maximum Height of Signs: No portion of any sign shall extend above the top of the building upon which it is located
- Freestanding signs shall be located ten or more feet from the nearest wall of a principal structure and shall not project over the roof of any structure



A city-wide system of signs and site furnishings contributes to the community sense of identity.



Signage can promote tourism and community pride with information about the city's cultural heritage and historic sites.

Special Permits

Special use permits

- Bingo parlors
- Broadcasting towers greater than 70 ft. height
- Churches, synagogues, temple in area greater one acre
- Community centers owned by govt. agencies
- Dormitories, fraternity houses and sorority houses
- Group home, rehabilitation centers
- Helicopter landing facilities
- Hospitals
- Hotels
- 90 days or more duration: Outdoor amusement
- Park-for-hire surface parking lots
- Poolrooms, billiard parlors, amusement arcades
- Rooming houses and boardinghouses
- Single room occupancy residence
- Truck stop
- Transfer of development rights
- Shelter

Special administrative permits

- Broadcast Towers 70 feet or less in height
- Driveways located between the sidewalk and the building for childcare centers
- Small family care home
- 90 days or less duration: Outdoor amusement
- Outdoor displays of merchandise within supplemental zone
- Off-site parking and shared parking within 1,000 feet of a primary use
- Relocation of minimum open space requirements
- Two curb cuts along one street frontage on properties with a single street frontage greater than 30 feet
- Variation in residential open space requirements for buildings built before 1950
- Variations in fenestration requirements
- Reduction or transfer of parking requirements
- Variations in driveway requirements, surface parking lot landscaping and barrier, sidewalk and supplemental zone width, requirements
- Farmers Market

Future landscape

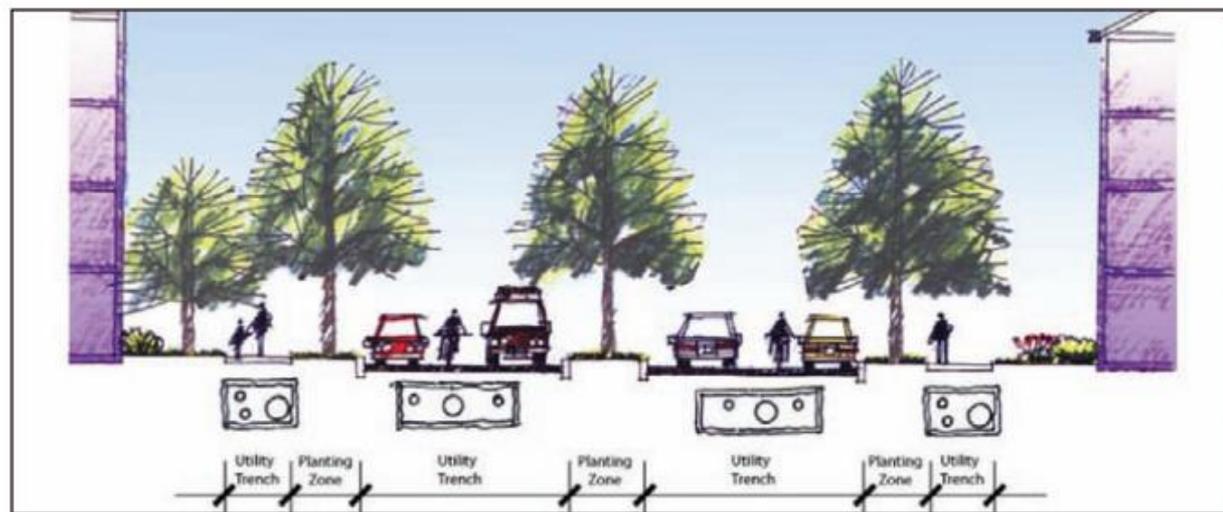


The intended community landscape character and the relationship of buildings to open space, streets and sidewalks



This perspective drawing illustrates how a public park will enhance the urban environment

Below ground utilities reduce clutter and eliminate conflicts between trees and utility lines



This perspective drawing illustrates how the urban streetscape should be clearly defined by buildings that conform to a uniform “build-to” line



On-street parking both parallel and angled, is convenient for the shoppers on mixed use street



Building corners provide opportunities to create community icons such as bell towers, projecting bay windows and porches

Rezoning Process

- **Legislation to rezone is introduced**
- NPU-Y Meeting
- Zoning Review Board (ZRB) Public Hearing
- Zoning Committee of City Council
- Adoption by Atlanta City Council
- Approval by the Mayor

■ For more information:

- Email: cjacks@atlantaga.gov
- Tel: 404.330.6145

■ Please visit our website

[Www.lakewoodcodistrict.weebly.com](http://www.lakewoodcodistrict.weebly.com)

- For Detail legislation, Online survey and PowerPoint presentations

Survey

Please complete the survey sheet for special permits and return it to the interns at the end of this meeting!

Thank you



Lakewood Heights

Thank You!

